

WHO WE ARE

With nearly 80 years of combined experience, NHDG and Losani Homes are leaders in building communities throughout the GTHA. The NHDG and Losani partnership stems back to 1998. The two companies have built new communities together over the years in Ancaster, Paris, Winona, and Oakville.

Both award-winning, family-run businesses have been recognized at the local level as well as Canada-wide. NHDG and Losani Homes share a renowned history in Hamilton and traditions of giving back to their community.



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WELCOME TO THIS DROP IN OPEN HOUSE

INFORMATION ABOUT THE PROPOSED REDEVELOPMENT IS ORGANIZED BY THE FOLLOWING TOPICS

Planning

Project Design and Architecture

Transportation and Parking

Heritage

Landscaped Open Space, Sustainability and Urban Design

PROJECT TEAM CONSULTANTS ARE AVAILABLE TO ANSWER QUESTIONS AND HEAR YOUR INPUT

HOW CAN YOU PROVIDE YOUR COMMENTS?

- By filling out the Comment Form and returning it to the Welcome Station
- By sharing your input at each station with project consultants
- By writing your comments on the flip chart paper available at each station
- By providing comments at 1284main.ca

You will be able to tour designated areas of the first and second floor of the original school building. For safety reasons, not all areas of the former school building are available for touring.

All comments received at this Open House will be included in a Feedback Report and provided to the City of Hamilton.

For details on how to formally participate in the planning process, please contact the City of Hamilton planner, Daniel Barnett at Daniel.Barnett@hamilton.ca or 905 546-2424 ext. 4445

For more information on this project, please contact Sarah Knoll, GSP Group at sknoll@gspgroup.ca or 289-778-1430

Visit 1284main.ca to view the plans and materials

Thank you for attending!



WHY IS THIS SITE BEING CONSIDERED FOR HOUSING?



In 2022, Ministry introduced a target of building **1.5 million** new homes in Ontario over the **next 10 years**. Hamilton has allocated **47,000 new homes by 2031**. Balancing both, a vision for the site that allows us to build for current housing demands, while also tying the scale of the design to the context of the retained heritage building and to the proposed perimeter townhouses.

The following policy frameworks have been considered for the development proposal:

PROVINCIAL
POLICY
STATEMENT

GROWTH
PLAN FOR
THE GREATER
GOLDEN
HORSESHOE

URBAN
HAMILTON
OFFICIAL
PLAN

CITY OF
HAMILTON
ZONING BY-
LAW

PART IV OF
THE ONTARIO
HERITAGE ACT



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WHAT IS BEING PROPOSED?

- The retention and enhancement of open space lands that front Main St. E and the south corners of the Site;
- Increasing greenspace on site, along with incorporating sustainability measures;
- The adaptive reuse of the original building on the Site for residential purposes; and
- The redevelopment of the remainder of the Site for residential purposes

Form	<p>3 storey townhomes</p> <p>4 storey stacked townhomes</p> <p>Existing retrofit building</p> <p>14 storey apartment buildings</p>	Total Units	975 with approximately 27% two and three bedrooms
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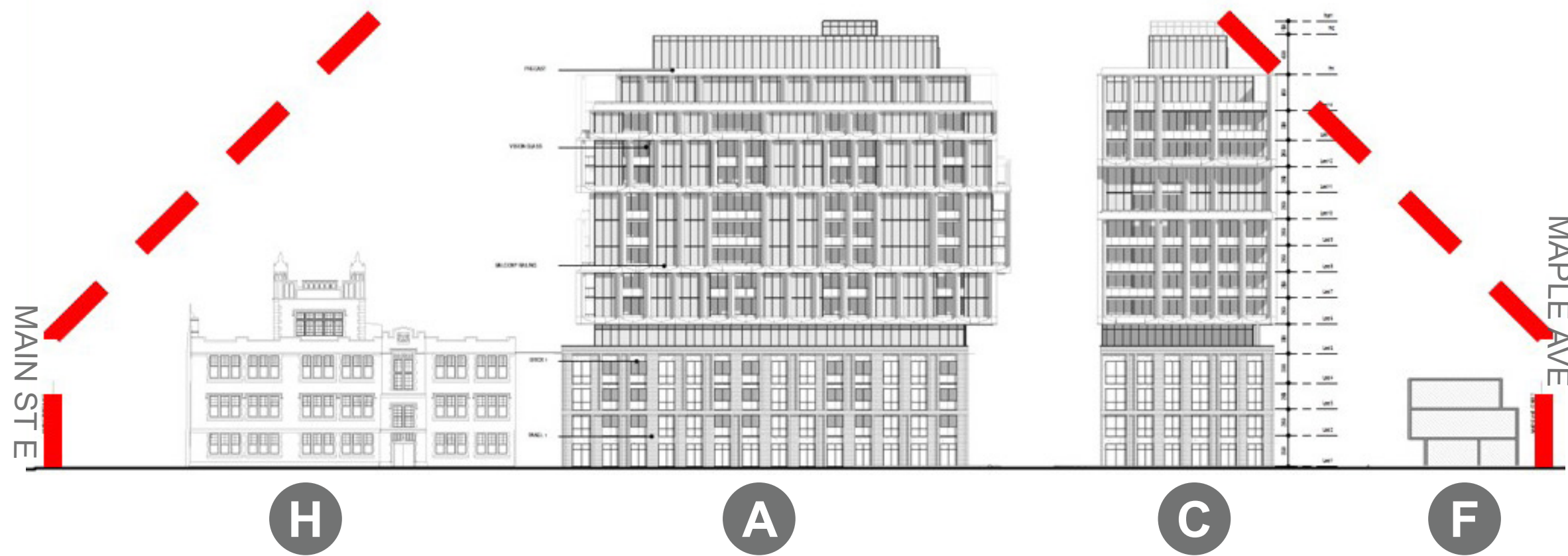
Unit Type	<p>17 Block townhouse units</p> <p>156 Stacked townhouse units</p> <p>802 Apartment units within: existing building (87) and new buildings (715)</p>	Parking	<p>Block Townhomes: 2 per unit tandem (garage/driveway) at grade</p> <p>Stacked Townhomes: 1.15 per unit located underground</p> <p>Apartments: 1.15 per unit located underground</p>
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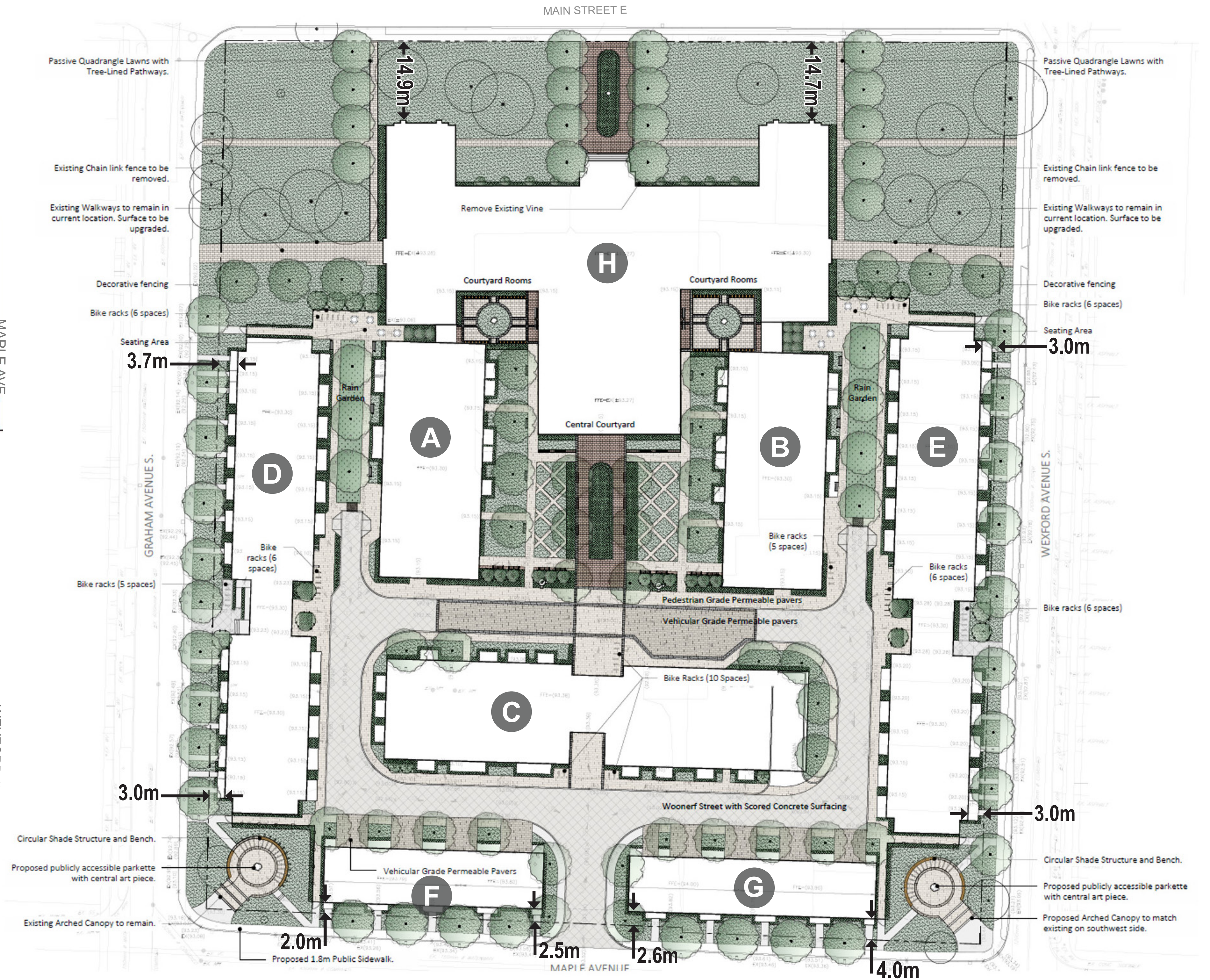
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PROPOSED DEVELOPMENT

ANGULAR PLANE NORTH-SOUTH



ANGULAR PLANE EAST-WEST



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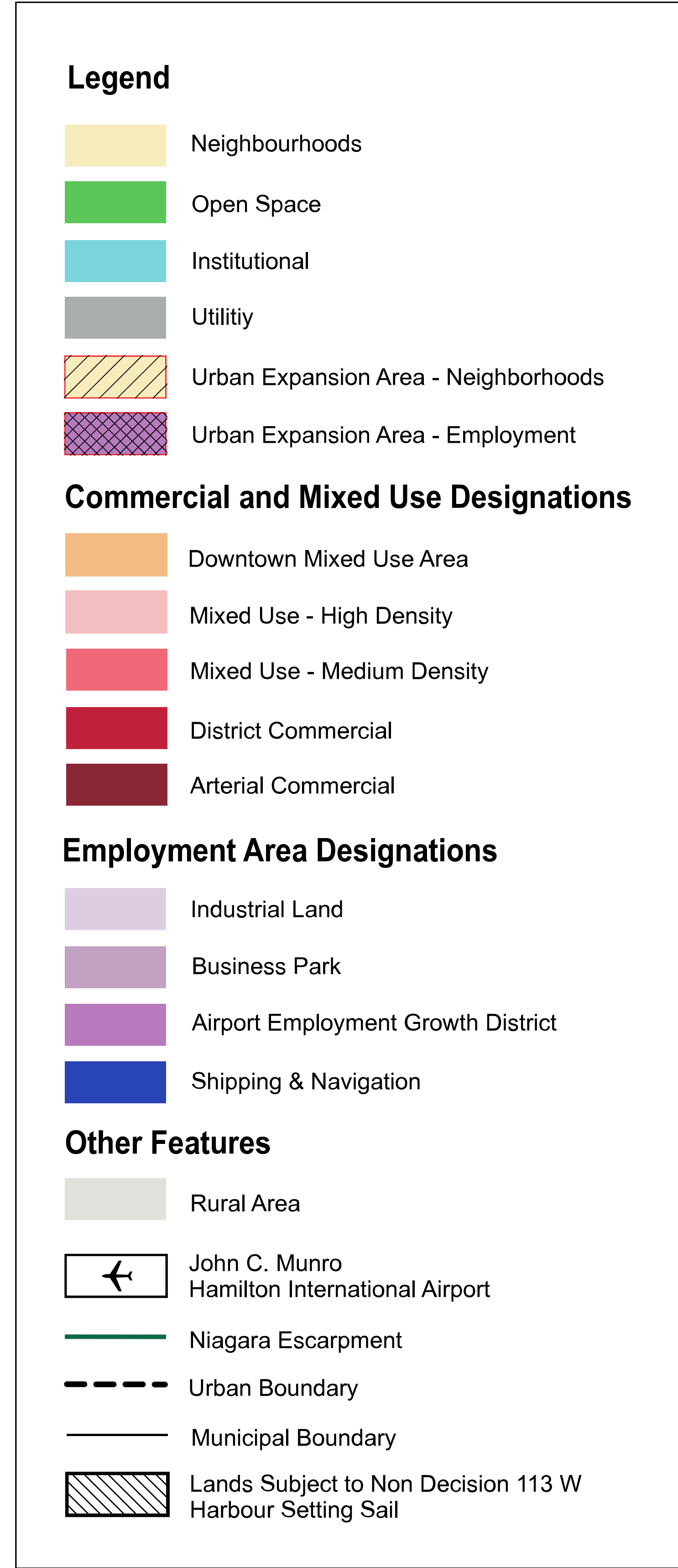
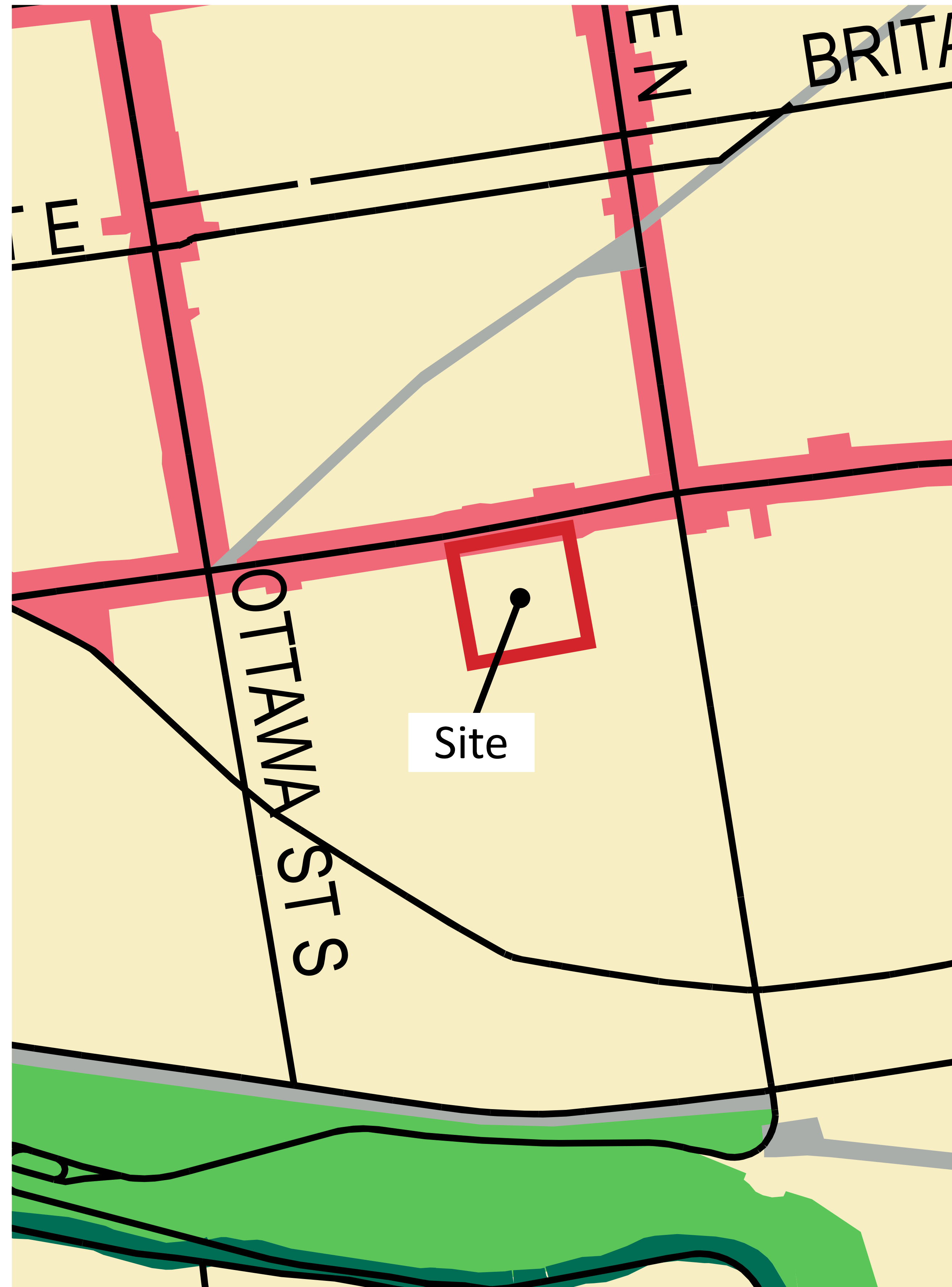
WHAT APPROVALS ARE BEING REQUESTED?

URBAN HAMILTON OFFICIAL PLAN AMENDMENT (UHOPA)

The front portion of the site is designated *Mixed Use - Medium Density* while the balance of the site is designated *Neighbourhoods*.

The UHOPA proposes to remove the dual designation, and designate the entirety of the site *Mixed Use - Medium Density* with a site specific modification to increase the maximum permitted height for the buildings interior to the Site an additional two storeys.

Note: Comments outstanding from City of Hamilton Planning Dept. which may result in changes to amendments.



WHAT APPROVALS ARE BEING REQUESTED?

ZONING BY-LAW AMENDMENT (ZBA)

The ZBA is required to rezone the site from the *Institutional (I2, 293) Zone*, to the *Transit Oriented Corridor Mixed Use Medium Density (TOC1) Zone* given that the institutional use ceases to exist on the property and the building is to be adaptively reused for residential purposes.

Site specific design regulations are amended relating to:

- principal pedestrian entrance location to buildings,
- ground floor elevation for residential units,
- driveway width,
- tandem parking,
- setbacks to Maple Ave and Main St E,
- minimum height for permitter buildings,
- maximum height for interior buildings.

Note: Comments outstanding from City of Hamilton Planning and Zoning Dept. which may result in changes to amendments.



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WHAT APPROVALS ARE BEING REQUESTED?

ZONING BY-LAW AMENDMENT (ZBA)

Site specific design regulations are amended relating to:

Building/Site Design:

- To reduce the finished floor elevation for ground floor dwelling units to 0.1 metres;
- To require principal entrances to face the street only for new buildings adjacent to the perimeter lot lines.
- To increase maximum driveway width from 6.0 metres to 7.5 metres as requested by Transportation Planning
- To permit tandem parking for block townhouses (driveway and garage space)

Setbacks:

- To decrease the minimum building setback along Maple Ave to 2 metres ;
- To increase the maximum building setback from Main St E to 14.7 metres (recognizing existing building);

Height:

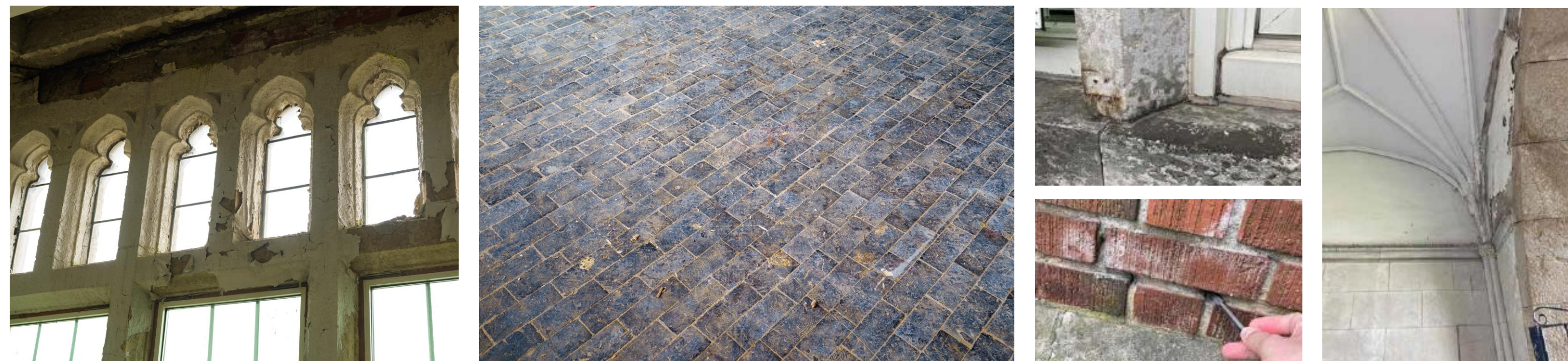
- To decrease the minimum building height from to 10m for the new perimeter buildings;
- To increase the maximum building height to 44.0 metres (14 storeys) for the new interior buildings;

Note: Comments outstanding from City of Hamilton Planning and Zoning Dept. which may result in changes to amendments.



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HERITAGE



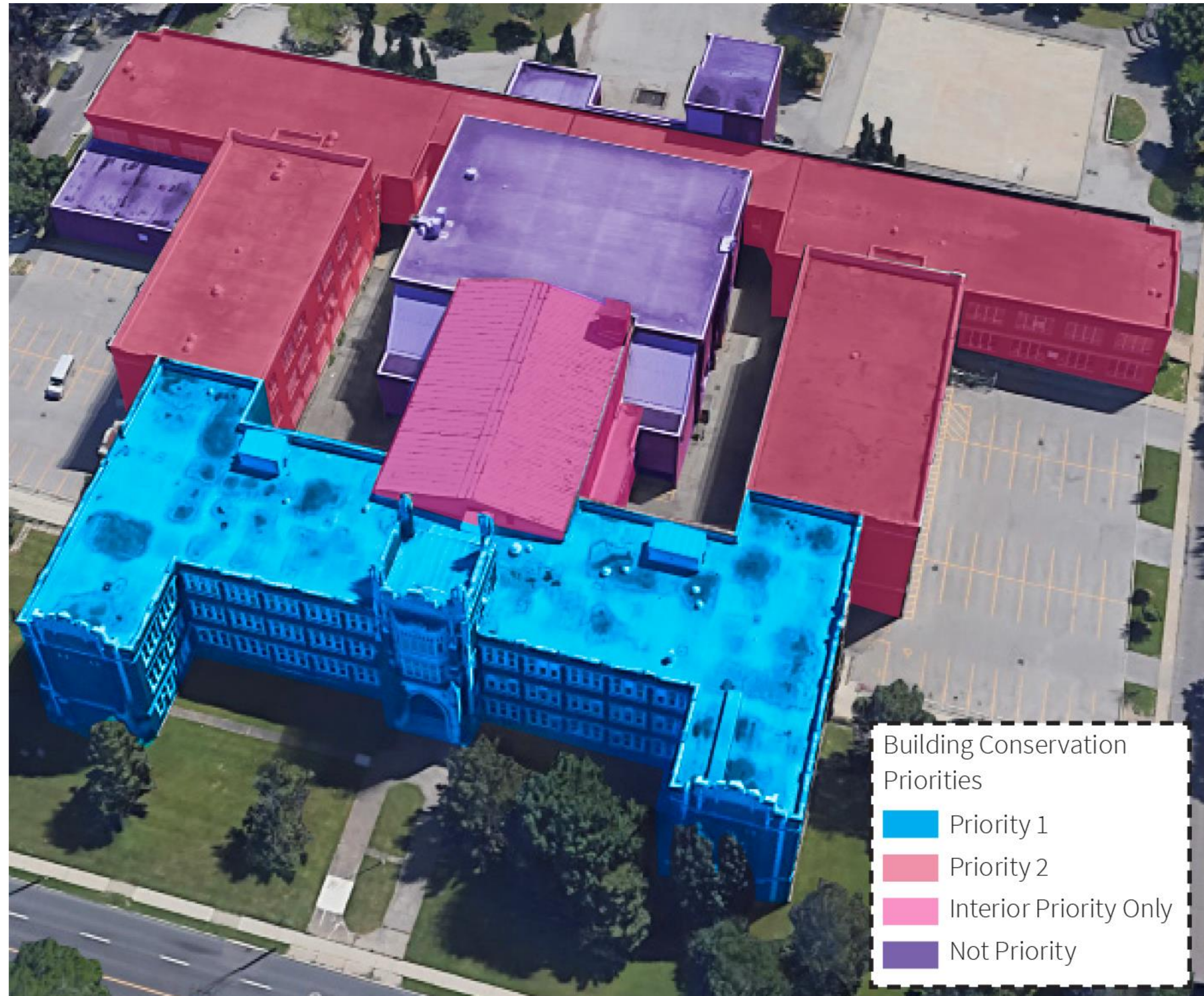
The site was designated under Part IV of the Ontario Heritage Act on April 9, 2014, by By-law 14-077. The property was designated following the proposed consolidation of HMDSB properties and the closure of Delta Secondary School, which operated from the site.



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ADAPTIVE REUSE

CONSERVATION DESIGN PARAMETERS



Highlight the retained 1924 School Building;

Reference the orientation of buildings in response to the school's historic evolution;

Transition massing to respond to and respect the historic building and the surrounding neighbourhood;

Reference historic materials and surrounding houses in the design; and

Offer a meaningful contribution to the public realm through landscaping and adaptive reuse.



OUR PROPOSED DESIGN

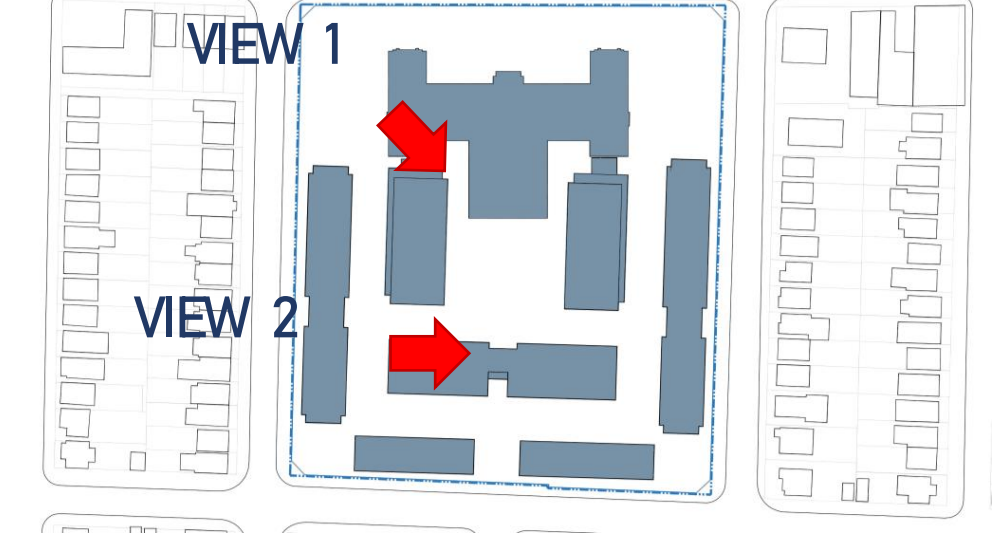


VIEW 1 - AERIAL VIEW LOOKING SOUTH-EAST



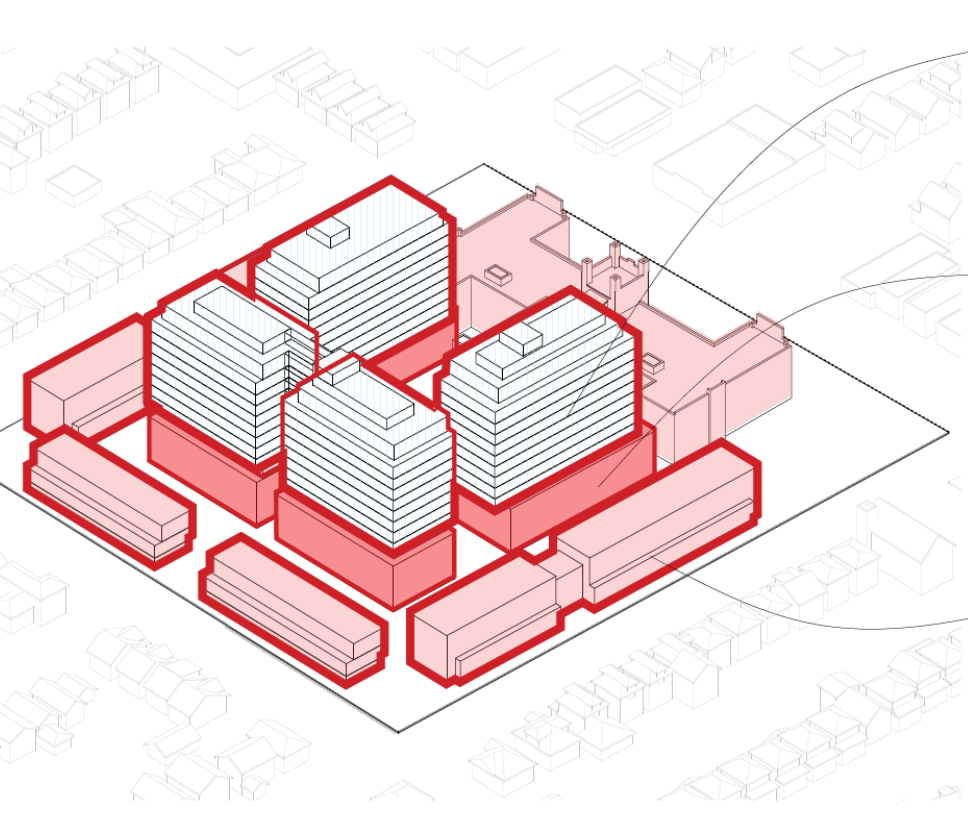
VIEW 2 - INTERNAL WOONERF

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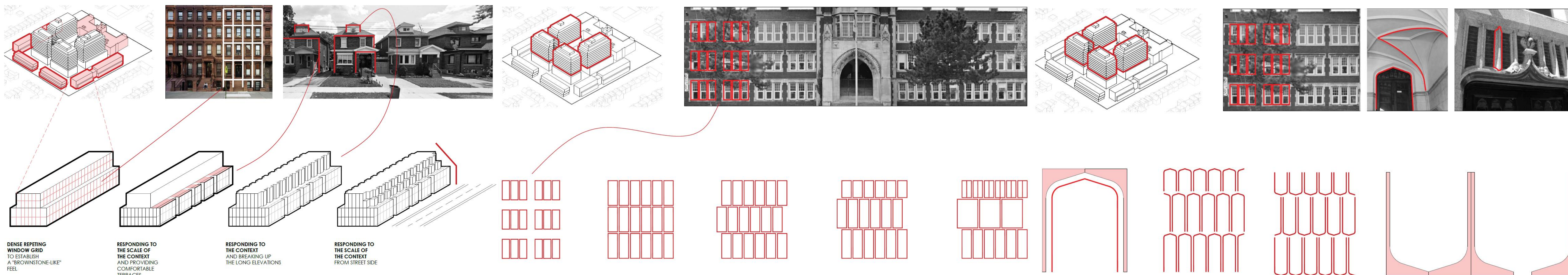


PROJECT COMPOSITION

CONCEPTUAL DESIGN COMPOSITION



- 03 MIDRISE "TOP"**
DESIGN DIRECTION & OBJECTIVES:
 - BE THE SCALE OF THE DESIGN TO THE CONTEXT.
 - BREAKUP THE MASS.
 - PROVIDE A DESIGN TO WORK IN 2 SCALES:
 A- CLOSE UP.
 B- AT A DISTANCE.
- 02 MIDRISE "PODIUM" CORE**
DESIGN DIRECTION & OBJECTIVES:
 - BE THE SCALE OF THE DESIGN TO THE CONTEXT OF THE REHABERED HERITAGE BUILDING AND TO THE PROPOSED PERIMETER TOWNHOUSES.
 - TRANSLATE THE MATERIALITY OF THE CONTEXT TO A NEW DESIGN.
 - FRAME THE INTERNAL OPEN SPACES.
- 01 TOWNHOUSE PERIMETER**
DESIGN DIRECTION & OBJECTIVES:
 - BE THE SCALE OF THE DESIGN TO THE CONTEXT OF THE RESIDENTIAL LOW RISE BUILDINGS.
 - ESTABLISH AN ATTRACTIVE STREET PRESENCE.
 - REINTERPRET HISTORICAL DESIGN STRATEGIES TO ESTABLISH A MODERN BROWNSTONE FEEL.



DENSE REPEATING WINDOW GRID TO ESTABLISH A "BROWNSTONE-LIKE" FEEL

RESPONDING TO THE SCALE OF THE CONTEXT AND PROVIDING COMFORTABLE TERRACES

RESPONDING TO THE CONTEXT AND BREAKING UP THE LONG ELEVATIONS

RESPONDING TO THE SCALE OF THE CONTEXT FROM STREET SIDE

PROJECT RENDERINGS



VIEW 3 - INTERNAL COURTYARD

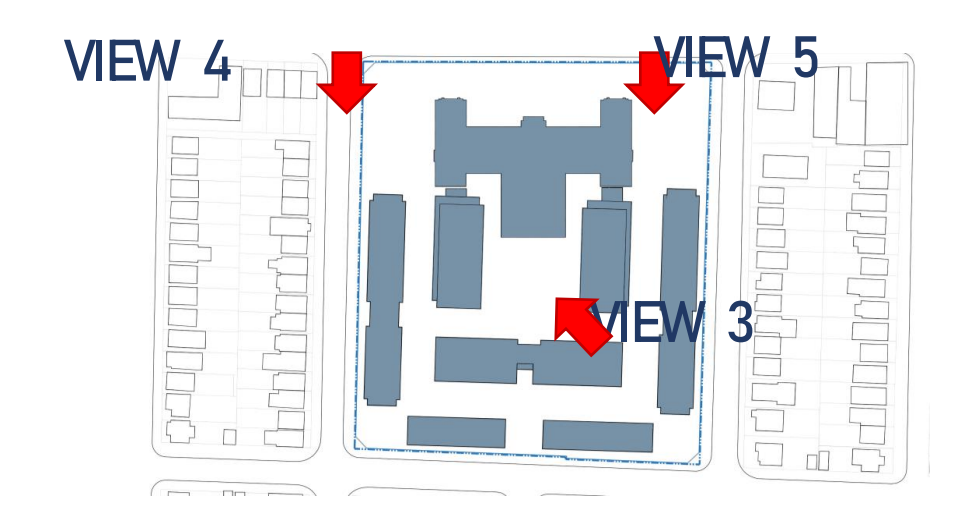
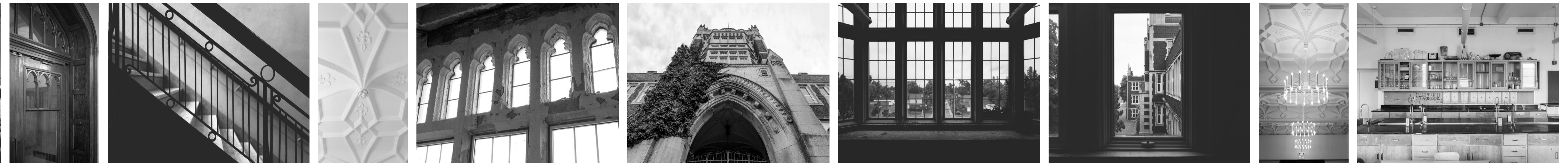


VIEW 4 - GRAHAM AVE

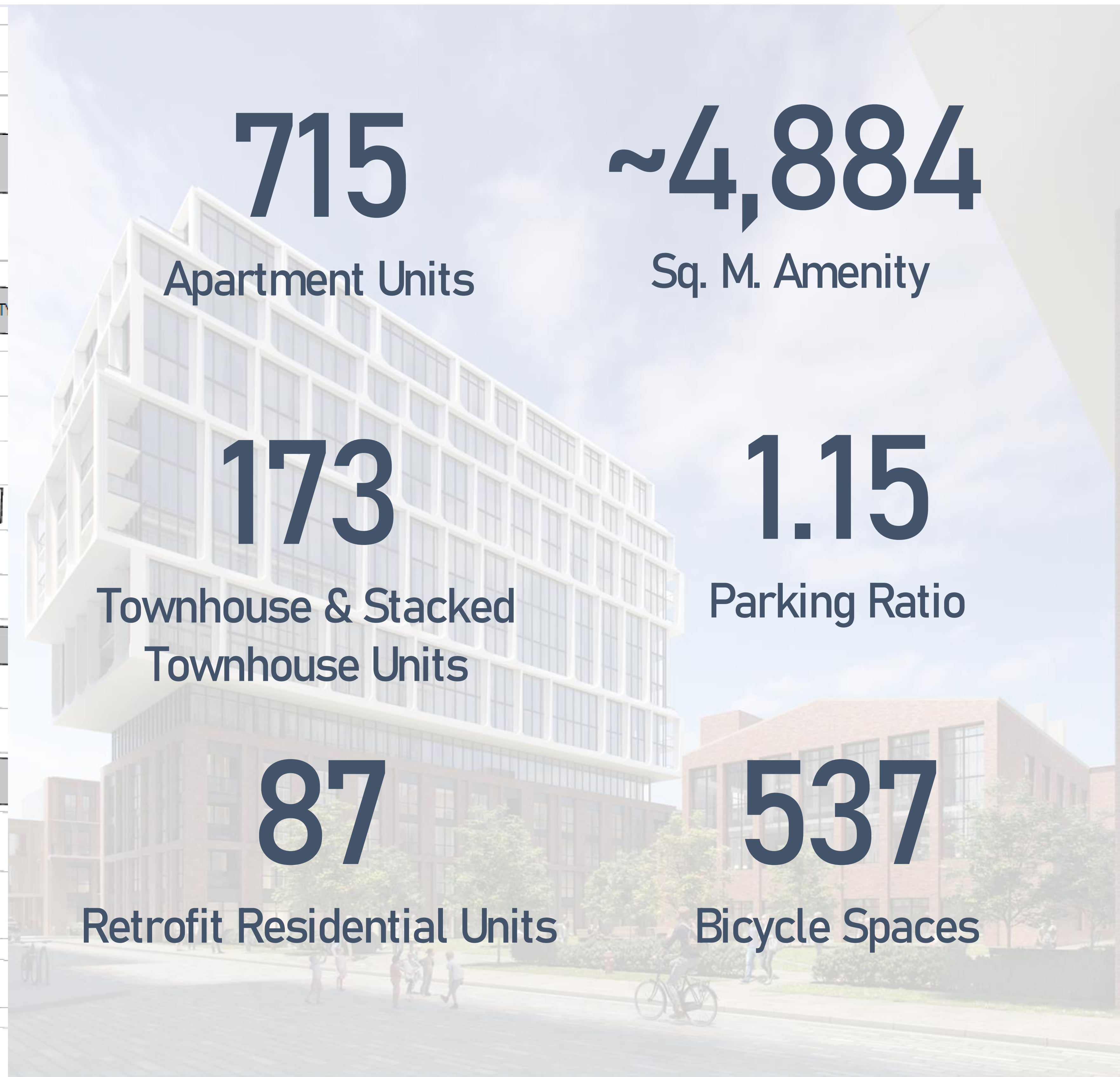
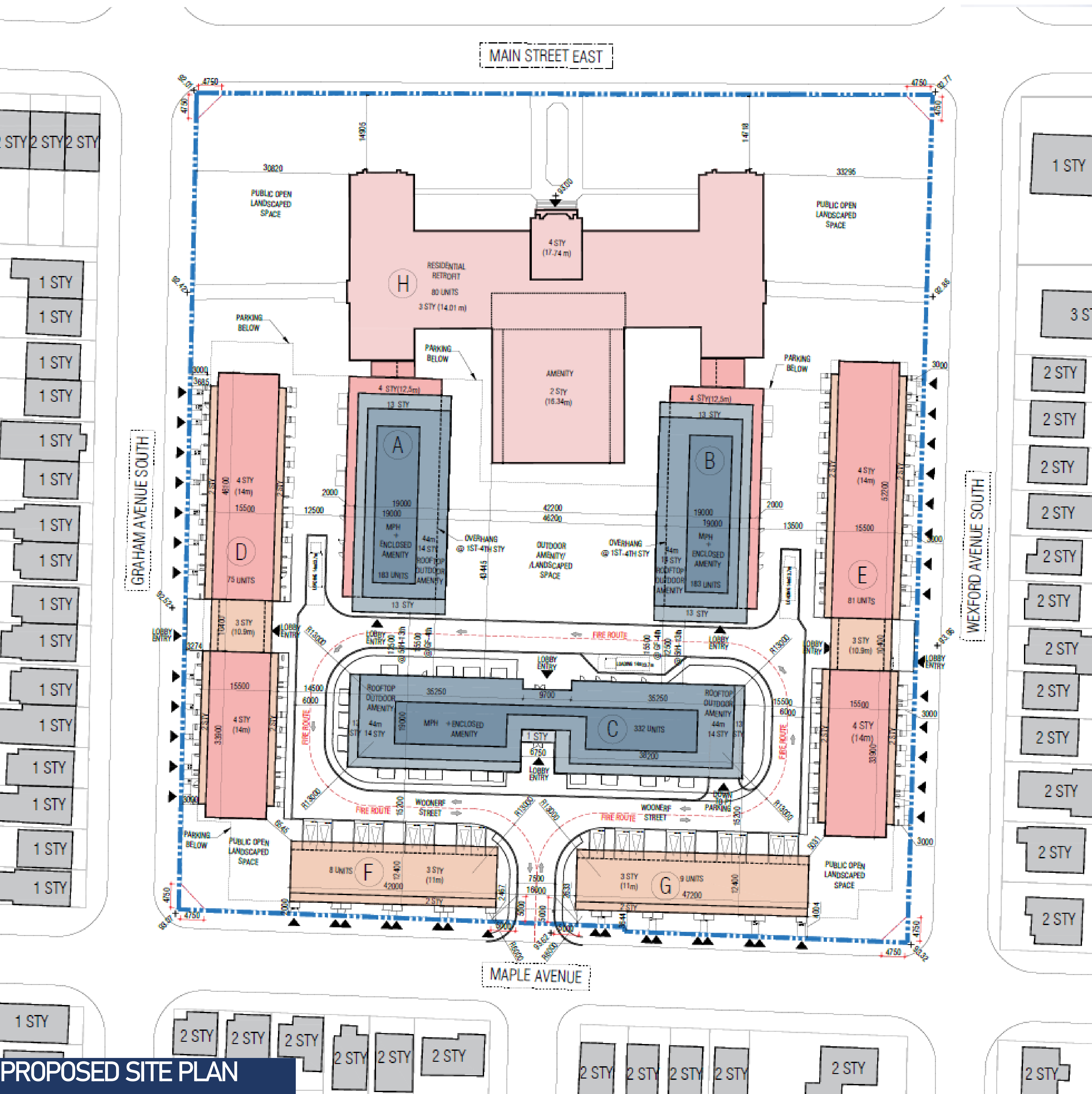


VIEW 5 - INTERNAL WALKWAYS

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PROPOSED SITE BREAKDOWN



VIEWS FROM YOUR STREET

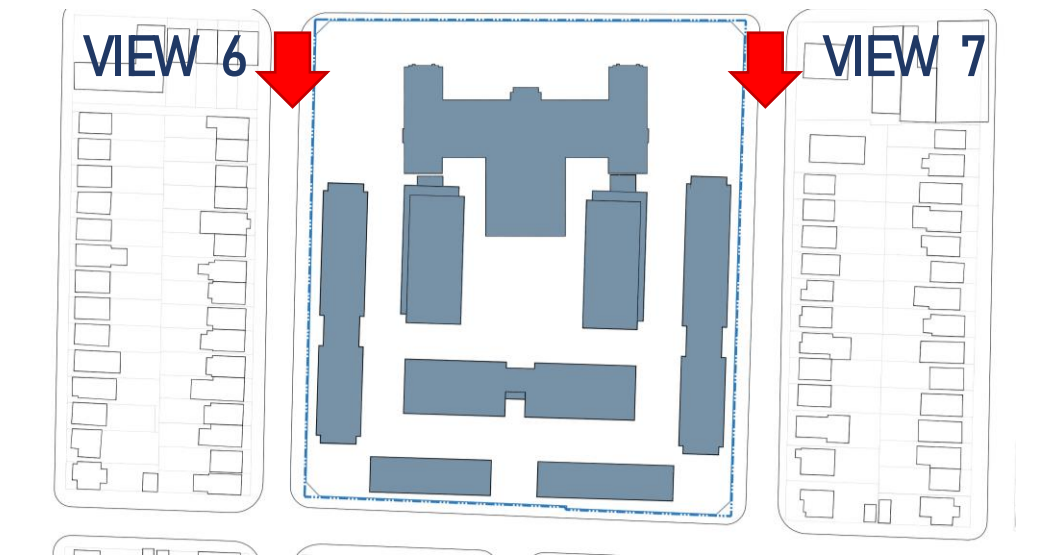


VIEW 6 - GRAHAM AVE S FROM MAIN ST

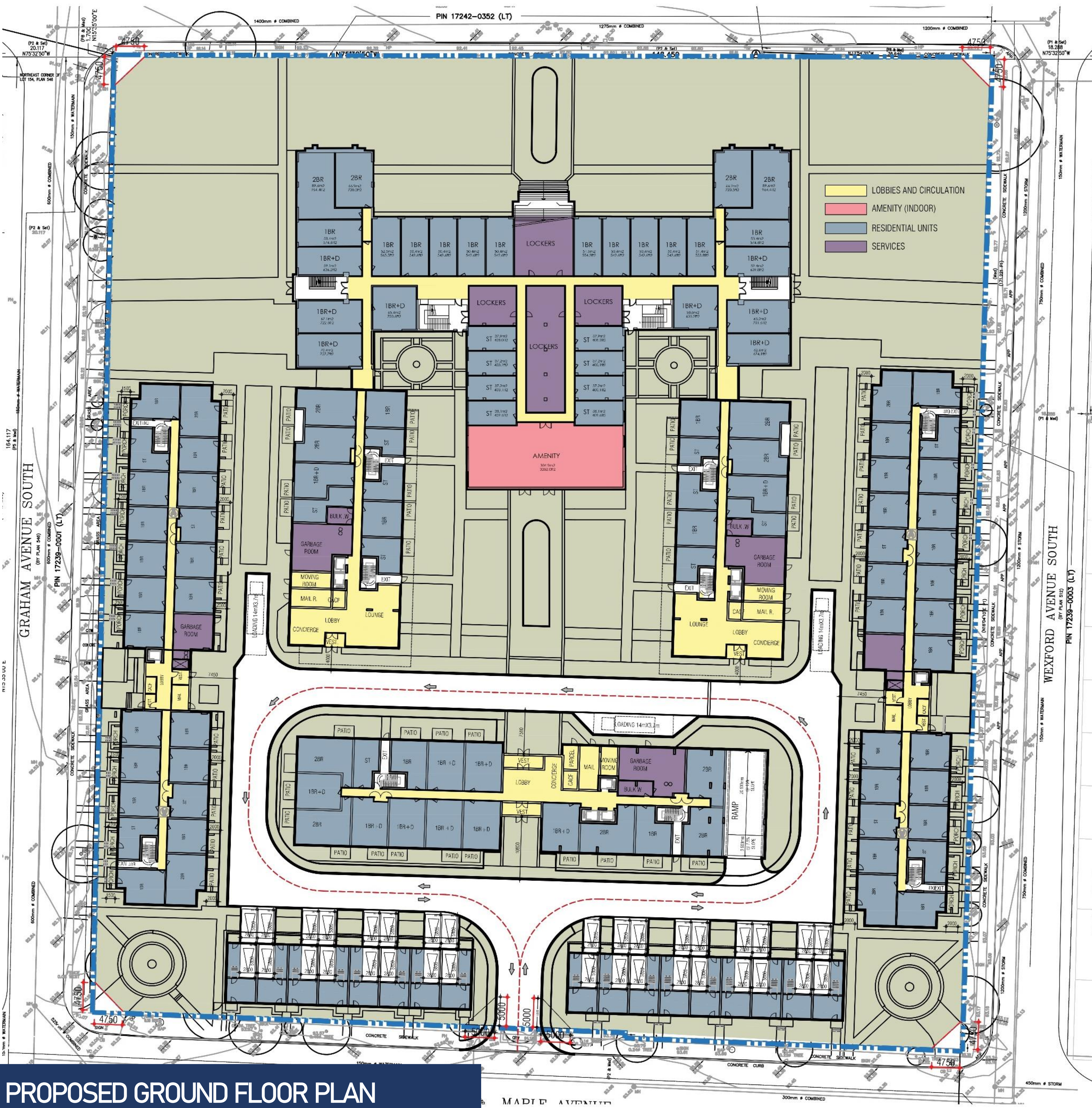


VIEW 7 - WEXFORD AVE S FROM MAIN ST

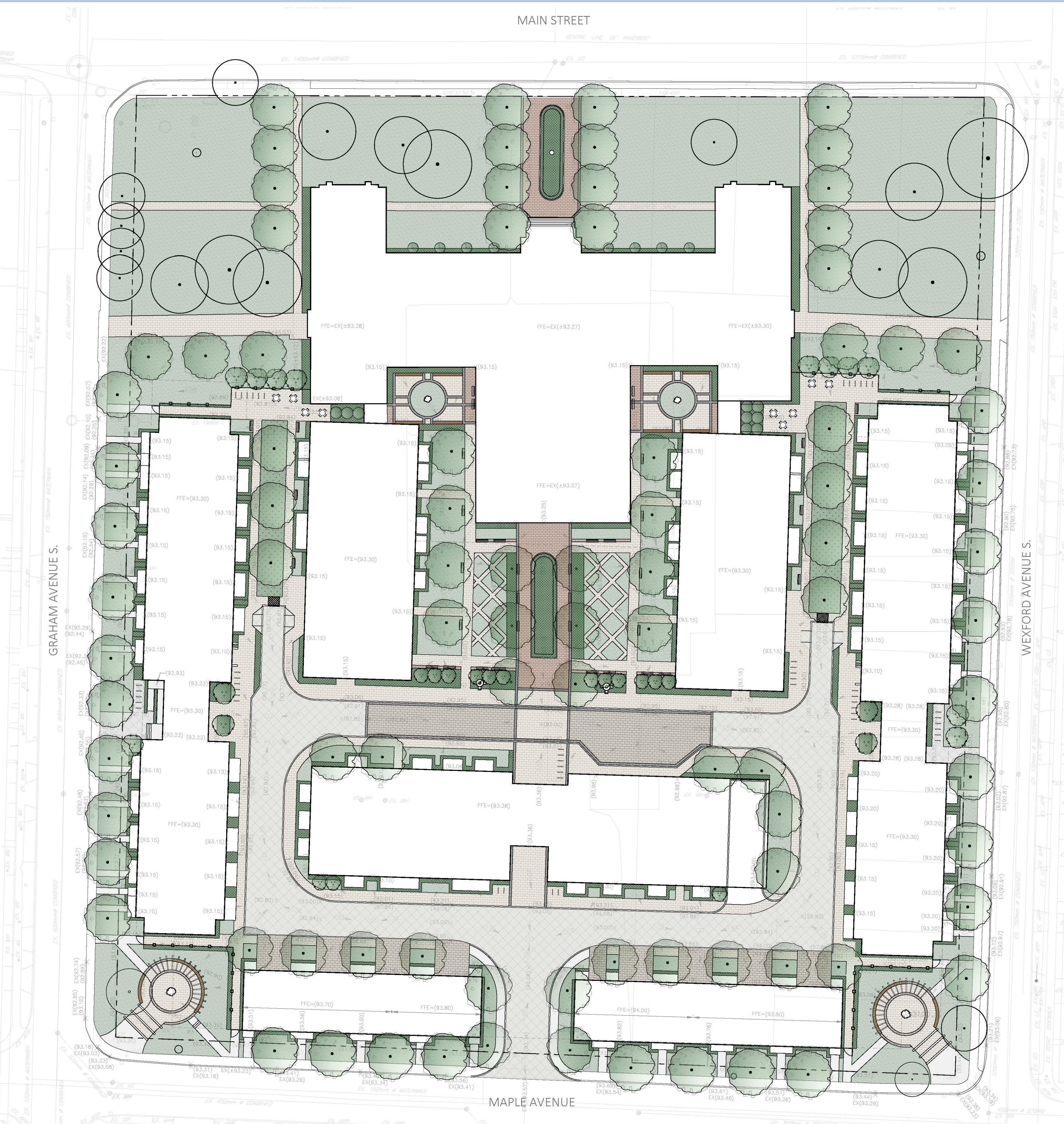
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PROPOSED UNIT BREAKDOWN



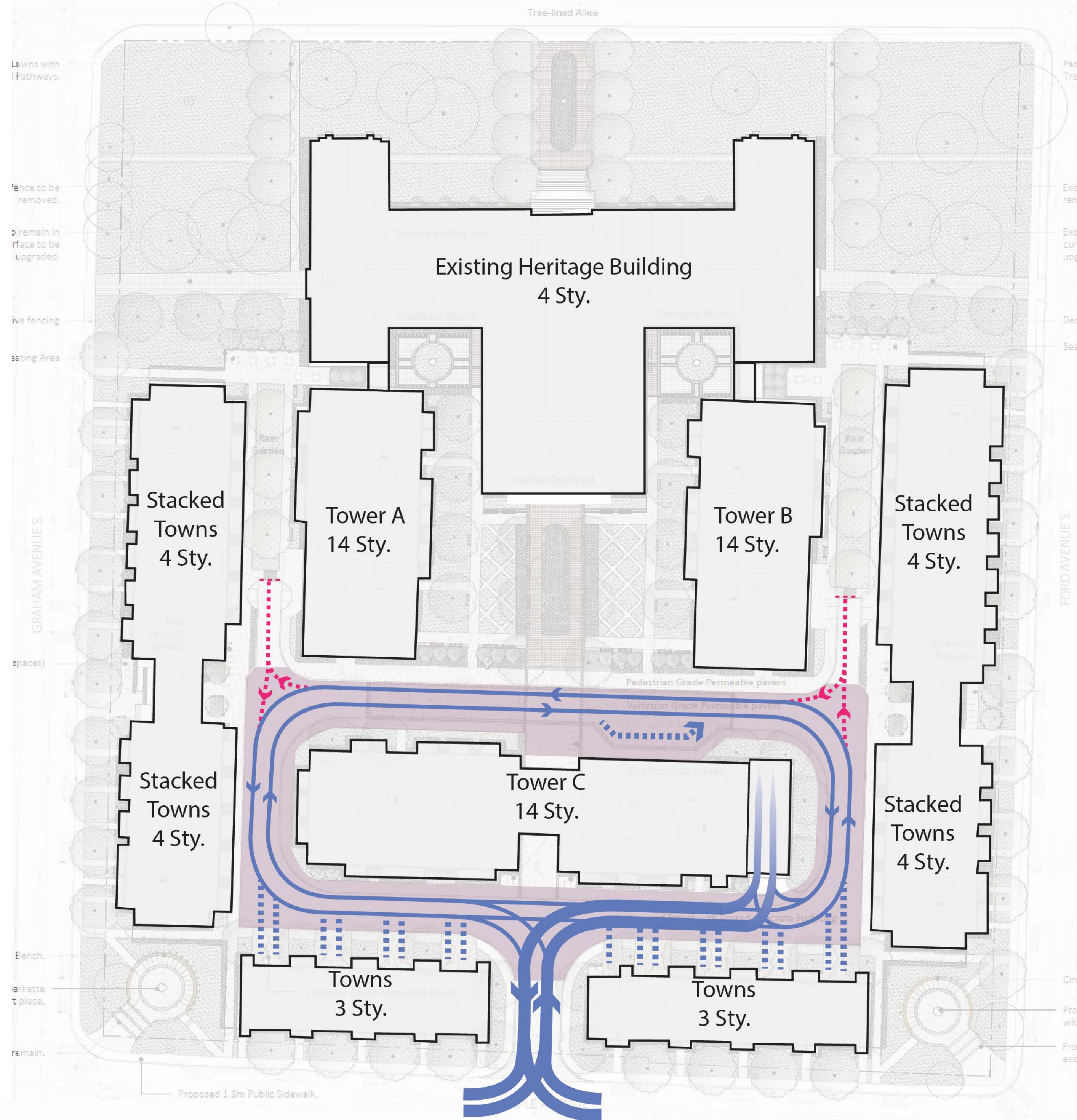
PROPOSED LANDSCAPE & OPEN SPACE



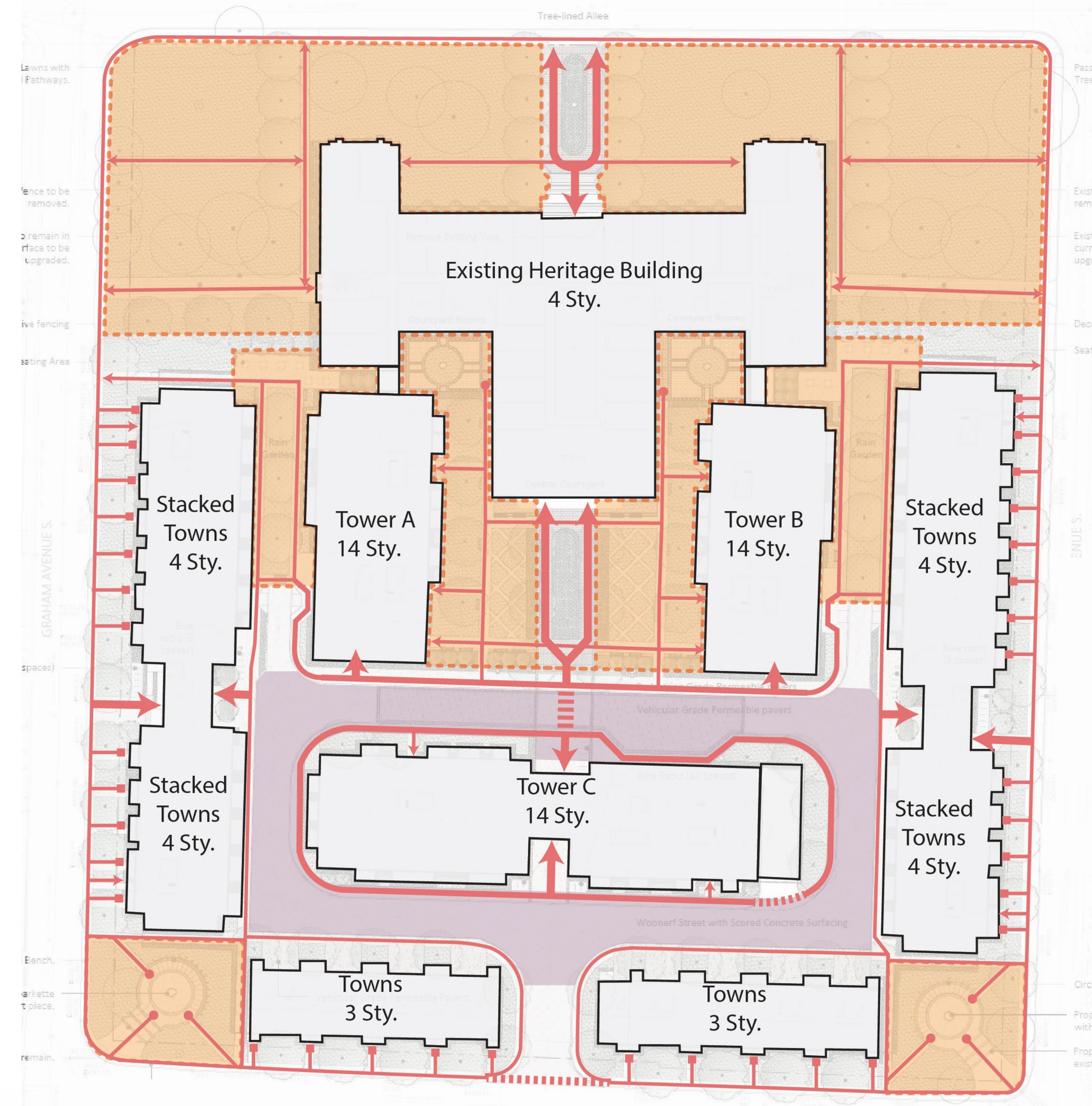
PROPOSED PUBLIC PARKETTE AT GRAHAM AVE S AND MAPLE AVE



SITE CIRCULATION



- Legend**
- Shared Pedestrian / Vehicular
 - Vehicular Circulation Routes
 - Loading Area

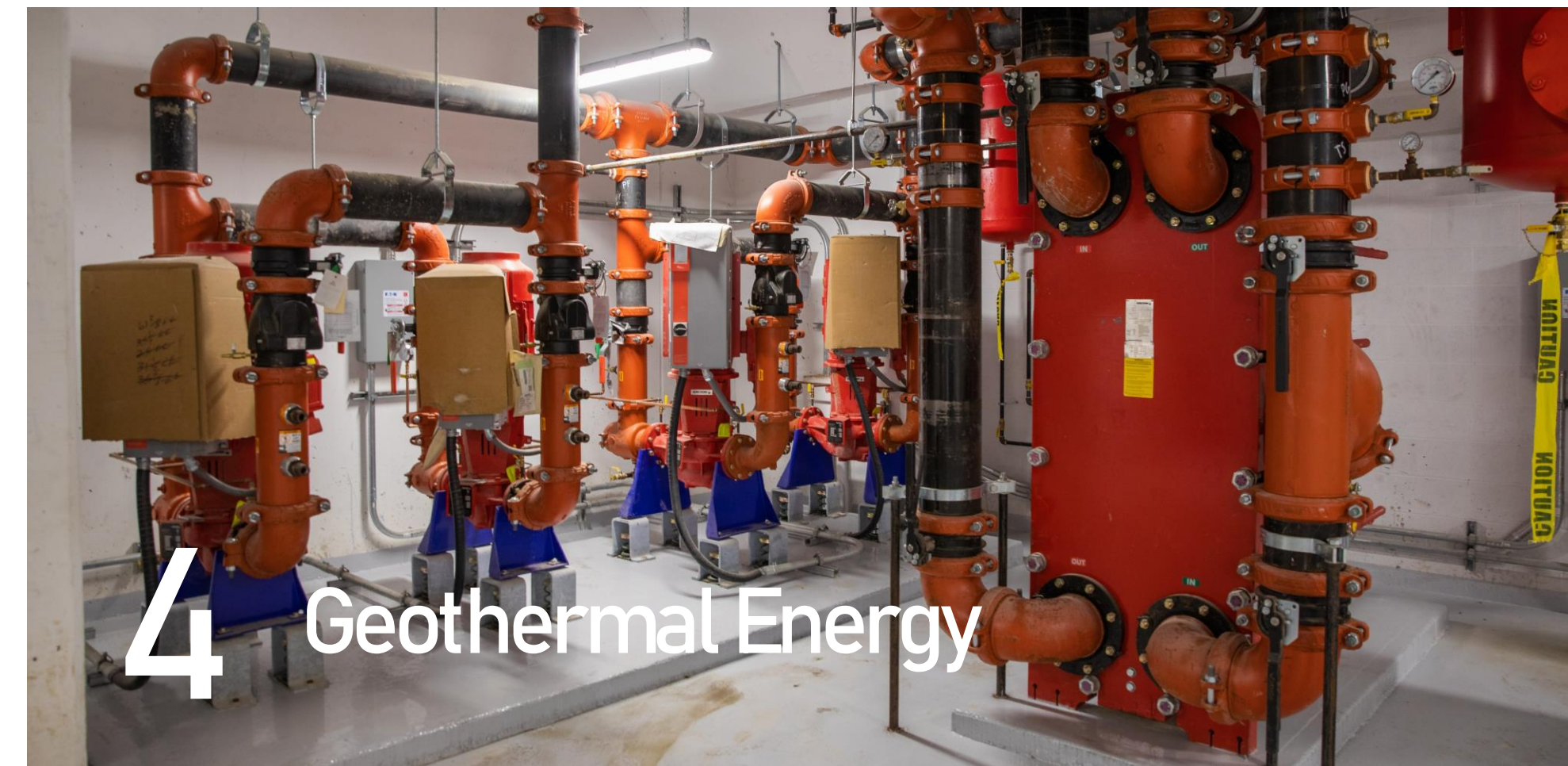


- Legend**
- Park / Amenity Space
 - Shared Pedestrian / Vehicular
 - Pedestrian Circulation Routes



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BUILDING A SUSTAINABLE COMMUNITY



Conceptual Images.

WHY DOES THIS MATTER TO YOU?

1. Reduces Urban Heat Island Effect
2. Use of Fewer Resources, Energy, and Emissions
3. Reduces Carbon Footprint
4. 60% Reduced Carbon Consumption
5. Minimize Impact on Ecosystems
6. Relieves Stress on Municipal Treatment Plants during Storm Events



CONNECTING HAMILTON – FUTURE LRT

14 km of sewer pipe to be replaced

16 km of watermain pipe to be replaced

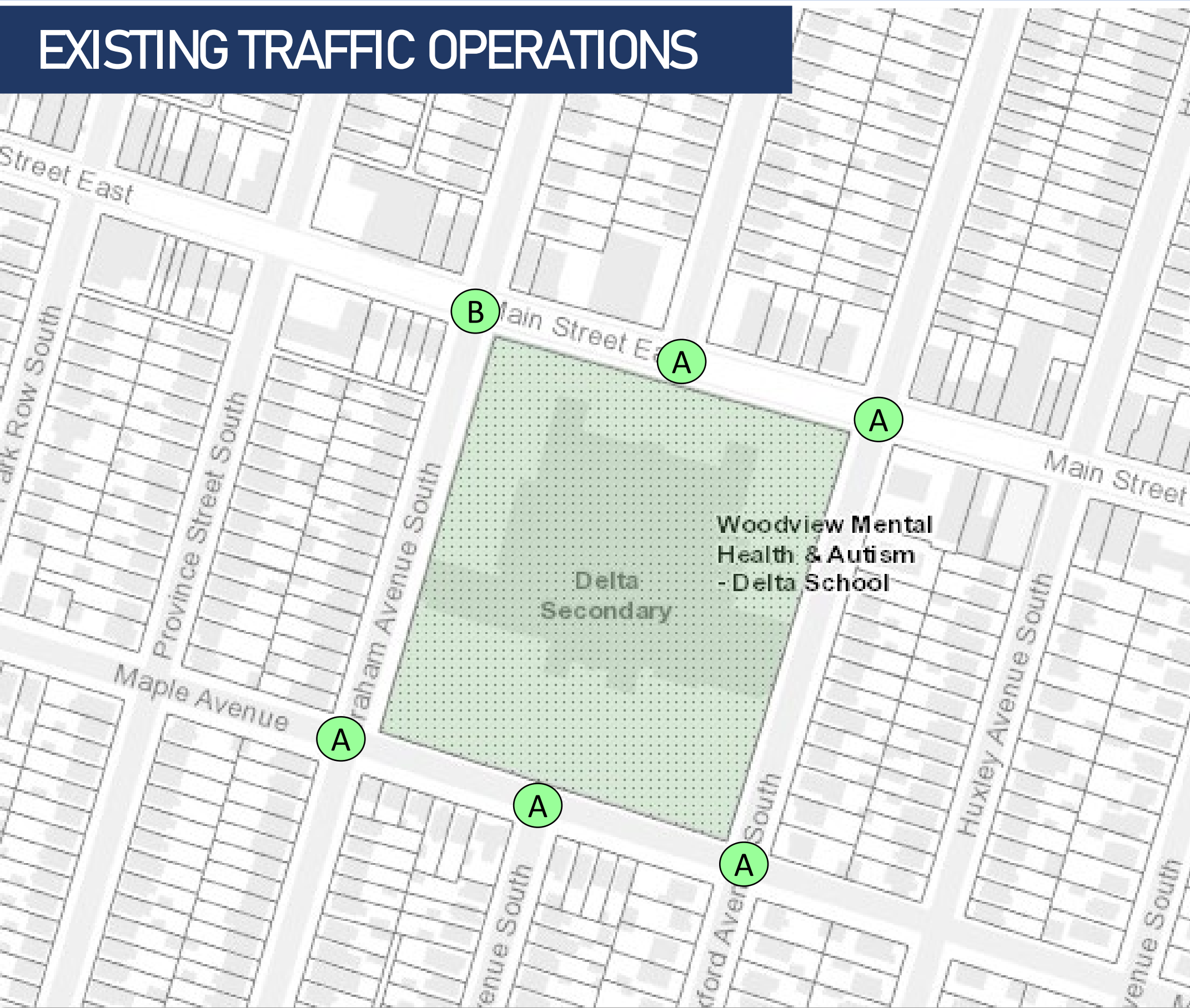
28 km of sidewalk replaced and upgraded to AODA standard

62 traffic signals replaced and upgraded with new technology and 14 km of road reconstruction

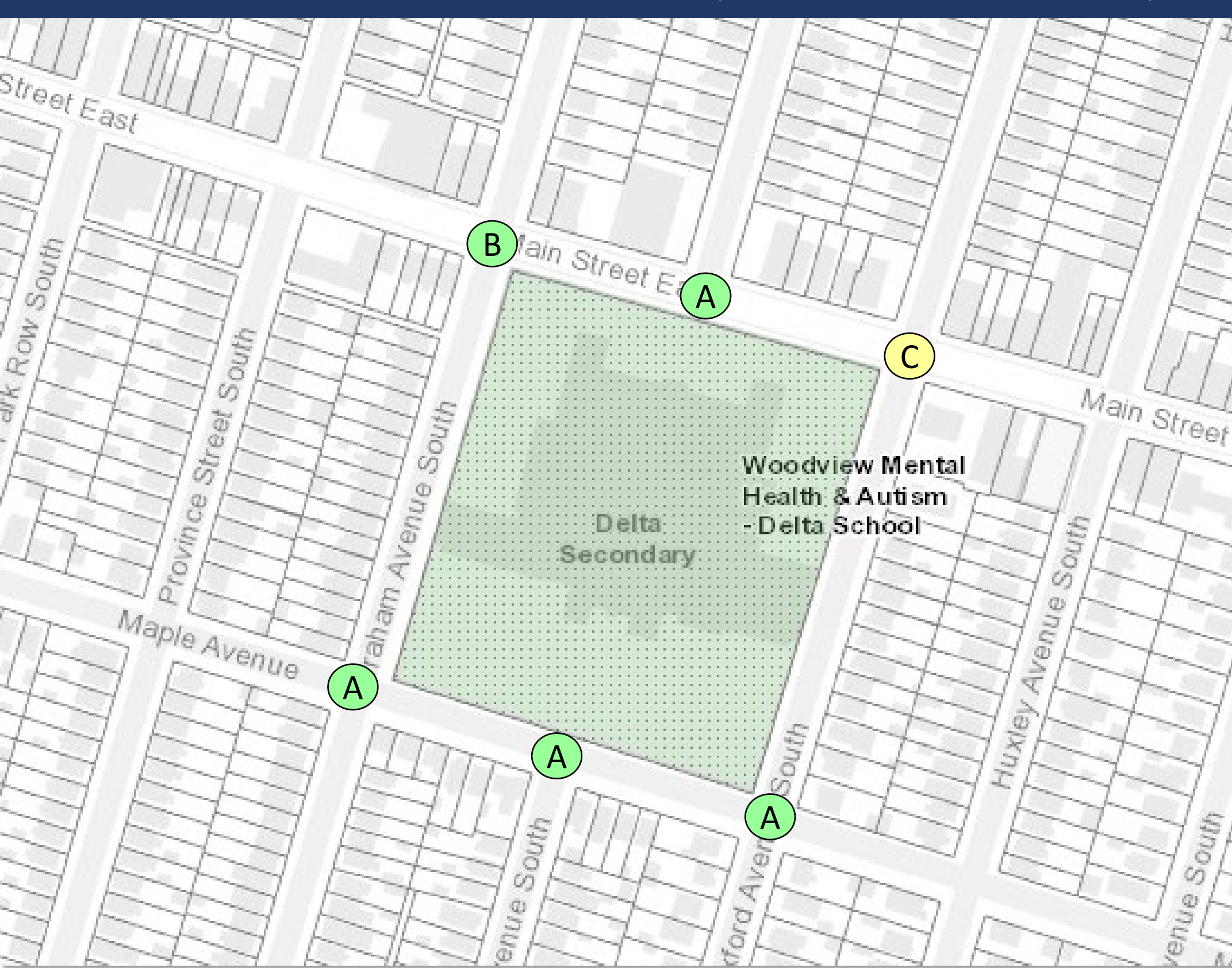


TRAFFIC OPERATIONS

EXISTING TRAFFIC OPERATIONS



FUTURE TRAFFIC OPERATIONS (SITE CONSTRUCTED)



- Traffic Analysis has been conducted on local area intersections
- Overall level of service is acceptable under existing conditions. With additional traffic added from the proposed development, no significant change in intersection operations is projected
- The development can be supported by the existing road network with minor impacts on intersection operations
- The City of Hamilton has reviewed the Transportation Study and accepts the findings
- The Applicant will contribute funds to the City of Hamilton for future consideration of traffic calming measures along local roadways

Level-of-Service	Description	Level-of-Service	Description
A	Little to no delay	D	Frequent queuing and delay
B	Minimal delay	E	Increased queuing and delay
C	Some queuing and delay	F	Significant queuing and delay



DELTA SECONDARY SCHOOL

TRANSPORTATION DEMAND MANAGEMENT

CYCLING

The development proposes providing 1,137 parking spaces for 975 units. The development also includes **490 long-term bicycle parking spaces**. Following the City's TDM Guide for Development and the proposed development parking, **49 short-term bicycle parking spaces** should be provided on site.

WALKING

The proposed site plan includes sidewalk connections linking the site to the municipal sidewalk on Main St E, Graham Ave S, Wexford Ave S, and Maple Ave.

TRANSIT

Existing transit stops located immediately north of the site at Main St E and Graham Ave N are expected to continue serving the area. Once the B-Line LRT is implemented, the closest stop will be Main St E at Kenilworth Ave, approximately 250 m east of the site.

PARKING

The proposed parking for the development is 1,137 parking spaces for 975 residential units, with a **parking rate of 1.17 per unit**. The proposed parking is within the recommended by-law rate of 1.0-1.25 spaces per unit.

TRAVEL PLANNING/ EDUCATIONS/ PROMOTION

Potential for wayfinding signage in the site to direct residents to active transportation facilities and transit stops.

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